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Date 16\_0920 | Subject Wood Village Town Center Zone Amendments

To Bridget Wiegart | From Marcy McInnelly, Erika Warhus

DRAFT REVISED OUTLINE OF TOWN CENTER ZONE: SECTION 235

New elements and sections that have moved locations are shown *in italics*. They retain their existing numbering system in this outline for clarity of comparison between the existing and proposed new outline. Text that has been crossed out indicates sections that have been moved to a new location or removed.

**235.500 Master Plan**

- A. *Purpose*
- B. *When Required*
- C. *Procedures*
  - 1. *Pre-Application Review of Development Plan*
  - 2. *Master Plan*
  - 3. *Changes and Modifications*
- D. *Required Contents*
  - 1. *Master Plan Diagram*
  - 2. *Master Plan Text*
  - 3. *Effect of Plan*

*Figure 235-1 Concept Plan Regulating Diagram*

**General Provisions**

235.100 Purpose

235.110 Other Zoning Regulations

**Use Regulations**

235.200 Primary Uses

- A. Uses Allowed Outright
- B. Conditional Uses
- C. Prohibited Uses

**Table 235-1 Uses in Town Center Zone**

235.210 Accessory Uses

235.275 Community Retail

- A. Purpose
- B. Requirement

235.285 Community Feature

- A. Purpose
- B. Requirement

235.290 Non-Retail Employment Uses

- A. Purpose

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- B. Requirement

~~235.295 Fairview Wood Village Town Center Non-Retail Employment Uses~~

~~A. Purpose~~

~~B. Requirement~~

**Development Standards**

235.300 Purpose

*Table 235-2 Town Center Street Types*

235.310 Lot Size and Dimensional Standards

- A. Minimum Lot Size & Dimension

235.320 Building Setbacks

- A. Required Setbacks
- B. Extensions into required building setbacks
  - o Minor Projections Allowed

*Table 235-2 Development Standards in Town Center Zone*

235.330 Height

- A. Maximum Height
- B. Exemption to the Maximum Height
  - o Projections allowed

235.335 Floor Area Ratios

- A. Purpose
- B. FAR Standard
- C. Interim Parking
- D. Adequacy of Services

235.340 Required Landscaping

- A. Computation
- B. Watering
- C. Maintenance
- D. Height of Trees

235.345 *Pedestrian Standards*

(Expand and reorganize original "Pedestrian Standards" 235.345 to combine all of section 235.345 with elements from section 235.390 "Additional Regulations" and all of section 235.400 Town Center Mixed Use Architectural and Pedestrian Design Standards)

- A. Purpose
- B. General Standard

*Table 235-4 Building and Façade Standards by Street Type*

- C. Setback Treatment
- D. Building Orientation
  - o Cul-de-Sac
- E. Transit Stops and Transit Amenities

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F. Architectural Standards

- o Building Mass
- o New Buildings
- o Building Materials
- o Weather Protection
- o Roof-Mounted Equipment
- o Roofs
- o Pedestrian Orientation
- o Building Articulation

G. Signs

~~235.345 Pedestrian Standards~~

~~A. Purpose~~

~~B. General Standard (pedestrian circulation, sidewalk network, connectivity to primary entrances)~~

~~C. Additional Connections (connectivity to parking lot and amenities, public pedestrian connection every 330 ft)~~

~~D. Setback Treatment~~

~~E. Entrances~~

~~F. Building Orientation~~

~~G. Transit Stops~~

235.350 Accessory Structures

- A. Covered Accessory Structures
- B. Uncovered Accessory Structures

235.360 Exterior Display, Storage and Work Activities

- A. Exterior Display
- B. Exterior Storage
- C. Exterior Work Activities
- D. Paving

Table 235-3 Exterior Display Setbacks and Landscaping in Town Center Zone

235.370 Open Area Requirement

- A. Purpose
- B. Required Amount of Open Space
- C. Standards
- D. Application of the Requirement

235.371 Gateway Requirement

235.375 Transportation System Standards

- A. Regional Transportation Plan Standards
- B. Conditions

235.380 Fences

- A. Types of Fences
- B. Location and Height Limits

235.390 Additional Regulations

- A. Demolitions
- B. Design Review

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- C. Parking and Loading
- D. Public Access
- E. Sale or Conveyance Prohibited
- ~~F. Signs~~
- G. Solid Waste, Mixed and Recyclables Storage
- H. Uncontained hazardous materials are prohibited
- ~~I. Street Standards~~
- ~~J. Bicycle Path Connections~~

~~235.400 Town Center Mixed Use Architectural and Pedestrian Design Standards~~

~~Purpose and Applicability~~

~~A. Architectural Standards~~

- ~~1. Building Mass~~
- ~~2. New Buildings~~
- ~~3. Building Block Layout~~
- ~~4. Building Orientation~~
- ~~5. Building Materials (and colors)~~
- ~~6. Roof mounted Equipment~~
- ~~7. Roofs~~
- ~~8. Pedestrian Orientation~~
- ~~9. Compatibility~~
- ~~10. Human Scale~~

~~C. Pedestrian and Transit Amenities~~

~~235.500~~ **Master Plan**

- ~~D. Purpose~~
- ~~E. When Required~~
- ~~F. Procedures~~
  - ~~4. Pre Application Review of Development Plan~~
  - ~~5. Master Plan~~
  - ~~6. Changes and Modifications~~
- ~~D. Required Contents~~
  - ~~4. Master Plan Diagram~~
  - ~~5. Master Plan Text~~
  - ~~6. Effect of Plan~~

**SECTION 235  
TOWN CENTER ZONE**

**GENERAL PROVISIONS**

**235.100 Purpose.** This mixed use zone is specifically designed to encourage convenient living, working and shopping through a well-designed mixture of commercial, residential and employment uses. Special requirements ensure that housing will be built as part of larger commercial developments, and that open spaces and frequent pedestrian connections will be provided. The development standards provide flexibility while ensuring compatibility of adjacent uses. The zone encourages more dense housing within walking distance of commercial areas.

**Comment [ew1]:** Add lodging, entertainment to this sentence: "This mixed use zone is specifically designed to encourage convenient living, working and shopping through a well-designed mixture of commercial, lodging, entertainment, residential and employment uses."

**235.110 Other Zoning Regulations.** The regulations in this section state the uses and development standards for the base zone only. Additional regulations may apply to sites with overlay zones, or specific uses. Section 300 states these additional regulations. Section 400 indicates sites subject to overlay regulations. Special use regulations are referenced below.

**USE REGULATIONS**

**235.200 Primary Uses.**

- A. **Uses Allowed Outright.** The uses allowed in the Town Center zone are shown in Table 235-1 by the letter "Y". Uses are allowed if they comply with the development standards of the base zone and any additional regulations that apply to the use or the site. Additional regulations are listed in Section 300.
- B. **Conditional Uses.** The uses which are allowed upon approval through the conditional use review process are shown in Table 235-1 with the letters "CU". These uses must meet the conditional use approval criteria, the development standards and any additional regulations stated in Section 300.
- C. **Prohibited Uses.** Uses shown in Table 235-1 with the letter "N" are prohibited in the zone. Legally established uses and development which existed at the time this Code was adopted and would otherwise be prohibited, are subject to additional regulations in Section 640 Non-conforming Situations.

**TABLE 235-1**  
Uses in Town Center Zone

<b>RESIDENTIAL CATEGORY</b>	
Household Living	Y
Group Living	CU
Mixed-Use Residential/Commercial	Y
<b>COMMERCIAL CATEGORIES</b>	
Retail Sales and Service	Y
Marijuana Sales other than Medical	N
Office	Y
Quick Vehicle Servicing	N
Vehicle Repair	N
Self Service Storage	N
Commercial Recreation	CU
Major Event Entertainment	CU
<b>INDUSTRIAL CATEGORIES</b>	
Manufacturing and Production	CU
Light Industrial	Y
Warehouse and Freight Movement	N
Wholesale Sales	CU
Industrial Service	N
Waste Related	N
Marijuana Processor	N
Marijuana Wholesaler	N
Marijuana Producer	N
<b>INSTITUTIONAL CATEGORIES</b>	
Basic Utilities	Y
Community Service	CU
Parks and Open Space	Y
Schools	N
Colleges	N
Medical Centers	Y
Medical Marijuana Dispensary	N
Religious Institutions	N
Daycare	Y
Essential Service Provider	N
<b>OTHER CATEGORIES</b>	
Detention Facilities	N
Mining	N
Radio and TV Broadcast Facilities	CU
Rail Lines and Utility Corridors	CU

**Comment [ew2]:** Add "Lodging" as a use category and specify as an allowed outright use. Lodging will need to be added to definitions as well.

**Comment [ew3]:** Change to an allowed outright use.

**Comment [ew4]:** Remove use category. It no longer exists.

**Comment [ew5]:** Rename "Light Manufacturing" and adjust to conditional use.

Y = Yes, Permitted Use      N = No, Prohibited Use      CU = Conditional Use Review Required

**235.210 Accessory Uses.** Uses which are accessory to the permitted uses are allowed if they meet the development regulations of the base zone, specific regulations for the accessory use and any additional regulations stated in Section 300. Typical accessory uses are included in Section 710 - Use Categories.

**235.275 Community Retail.**

A. **Purpose.** The Community Retail requirement ensures that major retail development in the Town Center zone is not limited to large-scale retail uses, but includes a variety of retail uses in order to better satisfy the range of retail needs in a mixed-use community.

**B. Requirement.**

- (1) The Master Plan required by Section 235.500 shall provide for a range of retail uses by both size and type, and describe how the proposed range satisfies the purpose set forth in Subsection A of this section.
- (2) Where the applicant proposes to develop large-scale retail uses before developing smaller retail uses, the Master Plan for the site shall:
  - (a) Set aside a sufficient amount of land, in the same Master Plan area, to provide the smaller retail uses.
  - (b) Set forth a development schedule indicating the date by which the smaller retail will be constructed.
- (3) The owner of the land described in Subsection B(2)(a) may apply to the Planning Commission for an extension of the date established under Subsection B(2)(b). Consideration of such an extension shall be processed as a minor amendment under Section 235.500.C(3)(b).
- (4) The Planning Commission and City Council, in approving provisions for future construction of smaller retail uses, may impose other reasonable conditions to ensure that the smaller retail is built.

**235.285 Community Feature.**

A. **Purpose.** The community feature requirement ensures that a suitably sized feature, such as a park, plaza or community facility, will be included in every Master Planned area in the Town Center zone to serve as a neighborhood or community focal point, gathering place or amenity.

**B. Requirement.**

- (1) Every Master Plan required for property in the Town Center zone shall include a feature that is open and accessible to the public and that serves as a visual focal

point for surrounding development, as a gathering place, or as a community amenity.

- (2) The feature's size and characteristics shall be commensurate with the size of the area and the scale of development described in the Master Plan; but the property owner shall not be required to bear an unusual or burdensome cost.
- (3) The feature may be privately or publicly owned. If it is privately owned, the property owner may include the area occupied by the feature as part of the open area requirement imposed by Section 235.370, and shall be responsible for maintenance of the land and community feature. The property owner shall not be required to grant or give the area occupied by the feature to the City or other public body.
- (4) The City may acquire the land set aside for the community feature and construct a public park, public building or other public amenity with public funds. In such circumstances, the land occupied by the feature will not count toward the open area requirement of Section 235.370 and the City shall be responsible for maintenance of the land and community feature.
- (5) The Master Plan shall identify the location and desired characteristics of the community feature and shall set forth a schedule for development of the feature. The schedule may be amended through the minor amendment procedure of Section 235.500.C(3)(b).
- (6)

#### 235.290 Non-Retail Employment Uses.

A. **Purpose.** The Non-Retail Employment Use requirement promotes inclusion of office, light industrial, or other permitted non-retail employment uses, to provide employment opportunities in addition to retail employment in the commercial component of development in the Town Center zone.

B. **Requirement.**

- (1) Every Master Plan for property in the Town Center zone shall demonstrate that where practical the new floor area of non-residential development is designed to be used as, or is adaptable to, non-retail employment use, even if it is initially intended for retail use.
  - (a) To be adaptable to non-retail employment use, the development must not contain structural barriers to non-retail employment use. The applicant shall provide evidence that conversion to non-retail employment use is feasible.
  - (b) In addition, for that portion of non-residential development intended to be used as, or adaptable to, office use, individual structures must have ground floor windows along substantial portions of the length of exterior building walls that abut sidewalks, plazas or other public open spaces. The windows must cover 25 percent of the ground level wall area up to 9 feet above grade on those walls to which the standard applies.

**Comment [ew6]:** Add Section 6: Where an approved master plan in the town center identifies the use of mobile food units or food carts, such uses shall be specifically authorized in the required community feature, and all lands associated with the use shall be considered part of the community feature.

**Comment [ew7]:** Eliminate the provisions for conversion to non-retail uses. Retain the "big box" limit but do not limit total retail. Lodging and entertainment uses should not be limited by this provision. "Non-Retail Employment Uses" means Light Industrial uses, Medical Center uses, Office uses, Daycare, Personal Service-oriented uses, entertainment-oriented uses, lodging uses, and repair-oriented uses.

**Comment [ew8]:** Delete section.

**Comment [ew9]:** Delete section.

- (2) Every Master Plan shall allocate 25 percent of the total planned new floor area of non-residential development for potential conversion to non-retail employment use.

Comment [ew10]: Delete section.

- (3) No more than three retail uses with a floor area of more than 60,000 square feet (“big box development”) are allowed in the Wood Village portion of the Fairview-Wood Village Town Center, and such large scale uses shall total no more than 362,000 square feet. The total floor area devoted to retail use shall not exceed 412,000 square feet.

Comment [ew11]: Change language: For this chapter, lodging and entertainment uses are classified as non-retail employment and use. No more than three retail uses with a floor area of more than 60,000 square feet (“big box development”) are allowed in the Wood Village portion of the Fairview-Wood Village Town Center. Lodging and entertainment uses shall not be limited by this provision. “Non-Retail Employment Uses” means Light Industrial uses, Medical Center uses, Office uses, Daycare, Personal Service-oriented uses, entertainment-oriented uses, lodging uses, and repair-oriented uses.

### 235.295 Fairview-Wood Village Town Center Non-Retail Employment Uses

A. **Purpose.** The Fairview-Wood Village Town Center Non-Retail Employment Use requirement ensures that the scale of retail development within this Town Center is appropriate and that the commercial component of development includes office, light industrial and other permitted non-retail employment uses to provide employment opportunities in addition to retail employment. The provisions of this section apply to development in the Wood Village portion of the Fairview-Wood Village Town Center in lieu of the provision of Section 235.290.

Comment [ew12]: Delete all of 235.295.

B. **Requirement.**

- (1) The Master Plan for the property in this Town Center zone shall demonstrate that 50 percent of the new floor area of non-residential development is designed to be used as or is adaptable to non-retail employment use, even if it is initially intended for retail use.
- (a) To be adaptable to non-retail employment use, the development must not contain structural barriers to non-retail employment use. The applicant shall provide evidence that conversion to non-retail employment use is feasible.
- (b) In addition, for that portion of non-residential development intended to be used as or adaptable to office use, individual structures must have ground floor windows along at least 50 percent of the length of exterior building walls that abut sidewalks, plazas or other public open spaces. The windows must cover 25 percent of the ground level wall area up to 9 feet above grade on those walls to which the standard applies.
- (2) No more than three retail uses with a floor area of more than 60,000 square feet (“big box development”) are allowed in the Wood Village portion of the Fairview-Wood Village Town Center, and such large scale retail uses shall total no more than 362,000 square feet. The total floor area devoted to retail use shall not exceed 412,000 square feet except as provided in paragraph (3).

- (3) No less than 125,000 square feet shall be devoted to Non-Retail Employment Uses. At least 50 percent of this Non-Retail Employment Use requirement must be satisfied from the following use categories: light industrial uses, medical center uses and office uses. If government offices or public meeting rooms are constructed as part of the Town Center development's community feature, the area devoted to those uses in the community feature shall be counted toward the 125,000 square feet non-retail office/light industrial requirement.
- (4) In the Wood Village portion of the Fairview-Wood Village Town Center, no less than 25 percent of total non-residential development, excluding the community feature, shall be Non-Retail Employment Uses. Following approval of the first increment of retail development and for each increment of retail thereafter, no further permits for retail shall be issued until the following "benchmarks" are satisfied:
  - (a) For the first 67 percent of the 500,000 square feet of non-residential development allowed under the Master Plan, the Non-Retail Employment Use requirement need not exceed 15% of the total non-residential development approved;
  - (b) Provided however, that the 25 percent Non-Retail Employment Use requirement for the entire development shall be met as part of the final 33 percent of the 500,000 square feet of non-residential development. (Ord. 15-2010)

**Definitions.** As used in this Section 235.295;

- (1) "Fairview-Wood Village Town Center" means the area on either side of 223rd Avenue designated in the Comprehensive Plans and zoning regulations of the cities of Fairview and Wood Village for mixed-use development. The "Wood Village Portion" of the Fairview-Wood Village Town Center consists of Tax Lots 800 and 200 in Section 34, T.1N, R.3E and Tax Lot 200 in Section 27, T.1N, R.3E, Willamette Meridian.
- (2) "Office uses" means the uses listed in Section 710.230 and branch banks, urgency medical care and veterinarian offices.
- (3) "New development" means development approved under Town Center zoning, including renovation of existing development if the existing development is being changed to a new use category as described in Section 710.010 to 710.540.

- (4) “Non-Retail Employment Uses” means Light Industrial uses, Medical Center uses, Office uses, Daycare, Personal Service-oriented uses as listed in Section 710.240.C(2), entertainment-oriented uses, as listed in Section 710.240.C(3) and repair-oriented uses listed in Section 710.240.C(4).

**DEVELOPMENT STANDARDS**

**235.300 Purpose.** Development standards are intended to promote site planning and design which will ensure that new land divisions and development will:

- Provide adequate flexibility to allow development which is compatible with the existing area, fits the topography of the site and allows architectural diversity.
- Provide adequate assurance of compatibility of adjacent uses within the mixed-use context, and compatibility between mixed-use development and adjoining existing uses.
- Comply with the Transportation Planning Rule and Regional Land Use and Transportation Goals.

**Town Center Street and Path Types Table**

Street Type	Target Speed <sup>1</sup>	Right-of-Way or Easement Width	Pavement Width	Vehicular Travel Lanes (number)	Travel Lane Width	Parking Lane Width	Bike Facilities	Sidewalk/ Path Width	Planting Strip Width / Furnishing Zone
<b>Town Center Main Street</b>	Under 20 mph	60 - 80 feet	36 – 56 feet	2 lanes (one in each direction)	10 feet maximum	8 - 18 feet <sup>2</sup>	Shared street	12 feet minimum	4 feet minimum (included within the 12-foot sidewalk width)
<b>Town Center Local Street</b>	Under 20 mph	56 feet	32 feet	2 lanes (one in each direction) with queuing	16 feet maximum	8 feet	Shared street	12 feet minimum	4 feet minimum (included within the 12-foot sidewalk width)
<b>Universal Street</b>	Under 20 mph	42 feet minimum	18 feet minimum	1 lane (one way street) or 2 lanes (one in each direction) with queuing	18 feet	0 - 18 feet <sup>3</sup>	Shared street	12 feet minimum	NA
<b>Service Street</b>	Under 20 mph	34 feet minimum	18 feet minimum	1 lane (one way street) or 2 lanes (one in each direction) with queuing	18 feet	0 - 18 feet <sup>3</sup>	Shared street	8 feet minimum	NA
<b>Trail / Multi-use Path</b>	NA	10 – 24 feet	6 -16 feet	NA	NA	NA	Shared or dedicated path	Varies	3 feet minimum

**Comment [ew13]:** New table: Town Center Street and Path Types to correspond with the new graphic: Concept Plan Regulating Diagram.

<sup>1</sup> Vehicular target speed as opposed to design speed, per CNU / ITE manual “Designing Walkable Urban Thoroughfares: A Context Sensitive Approach”

<sup>2</sup> Options for parking include parallel or diagonal or a combination

<sup>3</sup> Options for parking include none, parallel, diagonal or a combination

### **235.310 Lot Size and Dimensional Standards.**

- A. **Minimum Lot Size & Dimension.** The minimum lot size standards are set forth in Table 235-2. Creation of new lots is subject to the regulations of Section 450 - Subdivisions and Partitions. The lot size for all permitted or conditional uses shall be adequate to fulfill the applicable requirements stated below.

### **235.320 Building Setbacks.**

- A. **Required Setbacks.** The setbacks for front, rear and side yards are stated in Table 235-2.

#### **B. Extensions into required building setbacks.**

- (1) **Minor Projections Allowed.** Minor features of a building such as eaves, chimney, open fire escapes, bay windows, uncovered stairways and wheelchair ramps, uncovered decks or balconies, which are attached to the building may extend into a required building setback by no more than 20% of its depth. However, they may not be within 3' of a lotline.

**TABLE 235-2  
Development Standards in Town Center Zone**

STANDARD	COMMERCIAL USE CATEGORIES	RESIDENTIAL USE CATEGORIES <sup>(1)</sup>	MIXED USE	OTHER USE CATEGORIES
Minimum lot size - Min. lot density - Min. lot area - Min. lot width - Min. lot depth - Max. lot coverage (see section 235.310)	None	same as MR2 zone (see Tables 220-3 and 220-4)	None  0.4 FAR (40%) See pages 2-37 & 7-25	None
Maximum height (see section 235.330)	45 ft <sup>(2)</sup>	35 ft	85% 50 ft	45 ft <sup>(2)</sup>
Minimum setbacks - Front setback - Garage setback - Side setback - Rear setback (see section 235.320)	None None None <sup>(3)</sup> None <sup>(3)</sup>	10 ft <sup>(5)</sup> 20 ft <sup>(5)</sup> 5 ft 15 ft	0-10 ft 22 ft minimum when parallels street	10 ft None <sup>(3)</sup> None <sup>(3)</sup> None <sup>(3)</sup>
Minimum Landscaping (percent of site area)	15% <sup>(4)</sup>	Same as MR2 zone (see Table 220-4)	15 %	15% or 20% <sup>(4)</sup>

**Comment [ew14]:** Recommend reducing the use categories to two: Residential Use Categories and Mixed Use Categories (which would incorporate commercial and other) to simplify and clarify table and layout.

**Comment [ew15]:** Delete "see pages 2-37 & 7-25."

**Comment [ew16]:** Change to 115 ft.

**Comment [ew17]:** Change to 55 ft.

- (1) Residential uses included in the same structure as commercial uses are regulated as part of the commercial use for purposes of these development standards.
- (2) For sites abutting a light residential zone the maximum height is 25 feet within the first 25 feet from the lot line abutting the light residential zone.
- (3) Sites abutting a residential zone shall have a side or rear setback of 15 feet from the residential zone.
- (4) For sites abutting a residential zone, the entire depth of any required setback from the residential zone shall be landscaped.
- (5) This note applies to detached single-family dwelling units: garages shall not be closer to the street than the plane of the street-facing façade. For the purpose of this subsection, porches may be considered as part of the street-facing façade.

**Comment [ew18]:** Add sentence: One additional foot in height is permitted above 25' for each two (2') feet in lateral distance from abutting light residential zones up to the height limit for the zone.

**235.330 Height.**

- A. **Maximum Height.** The maximum height for all structures is stated in Table 235-2.
- B. **Exemption to the Maximum Height.**
  - (1) **Projections allowed.** Projections extending vertically for permitted buildings and uses, such as skylights, steeples, flagpoles, chimneys, and radio and television aerials, may be erected above the height limitations herein prescribed. Such structures shall not project more than 5' above the maximum permitted height.
  - (2) Utility power poles are exempt from the height limit.
  - (3) Commercial Recreation uses are allowed a height limit of 85 feet, subject to Conditional Use Permit approval in accordance with Section 620. Conditional Uses.

Comment [ew19]: Delete section.

**235.335 Floor Area Ratios.**

- A. **Purpose.** Floor area ratios (FAR) provide a means to match the potential density of uses with the desired character of the area and provision of public services. FAR is the amount of floor area in relation to the amount of site area, expressed in square feet.
- B. **FAR Standard.** A Master Plan for development in the Town Center zone shall show that a minimum 0.4 FAR can be achieved on the areas of the Master Planned site to be developed with non-residential uses at full build-out.
- C. **Interim Parking.** In early development phases, portions of the property set aside for non-residential uses at full build-out may be used for parking associated with uses built in early phases. However, no restrictions shall be placed on the property set aside for future non-residential use that will prevent development of those future uses.
- D. **Adequacy of Services.** The Master Plan must demonstrate that the transportation system is adequate or can be made adequate for initial phases of development. Higher levels of density achieving 0.4 FAR will not be allowed until the city and regional transportation systems can accommodate traffic generated by such levels of density with fully connected, multi-modal transportation facilities.

Comment [ew20]: "The Master Plan must demonstrate that the transportation system is adequate or can be made adequate for development. Higher levels of density achieving 0.4 FAR will be allowed when a Traffic Impact Analysis demonstrates that acceptable performance of the local street network can be achieved, with specific mitigation measures identified in the analysis."

**235.340 Required Landscaping.** The minimum landscaping requirements are stated in Table 235-2 and shall satisfy the following requirements:

- A. **Computation.** The minimum site area devoted to landscaping may be computed to include any required landscaping imposed by Section 350 - Parking and Loading.

- B. **Watering.** Provisions shall be made for watering planting areas where such care is required for survival.
- C. **Maintenance.** Required landscaping shall be continually maintained.
- D. **Height of Trees.** Maximum height of selected tree species shall be considered when planting under overhead utility lines.

**235.345 Pedestrian Standards.**

- A. **Purpose.** The pedestrian standards encourage a safe, attractive, and usable pedestrian circulation system, and ensure a direct and pleasant pedestrian connection between the street and buildings on the site.
- B. **General Standard.** A pedestrian circulation system must be provided, consisting of two parts: sidewalks on all public and private streets, and an onsite system which connects the street to the main entrance of the primary structure on each site. The circulation system must be hard-surfaced, and meet ADA standards. Where the system crosses driveways, parking areas, and loading areas, the system must be clearly identifiable, through the use of striping, elevation changes, speed bumps, a different paving material, or other similar method. Where a sidewalk is adjacent to commercial frontage, the width shall be at least eight feet along the entire frontage of the parcel or lot; otherwise the width shall be at least six feet. Larger sidewalk dimensions up to 20 feet are permitted where outdoor seating is desired by the applicant and high levels of pedestrian activity are expected. The Design Review Board may require planter strips between sidewalks and streets.
- C. **Additional Connections.** The applicant shall connect the pedestrian circulation system to other areas of the site, such as a parking lot and any pedestrian amenities, such as plazas, resting areas, and viewpoints, and, where practical, to pedestrian facilities on adjoining sites. Provide public pedestrian connections every 330 feet, where feasible.
- D. **Setback Treatment.** The land between a building and a street must be landscaped to at least the L1 level or hard-surfaced for use by pedestrians. This area may be counted towards any minimum landscaped area requirements. Vehicle areas and exterior display areas are exempt from this standard.
- E. **Entrances.** Building entrances shall incorporate roofs, porches, alcoves, porticoes or awnings that protect pedestrians from rain and wind.
- F. **Building Orientation.**
  - (1) All buildings shall have at least one main entrance oriented toward an abutting street, pedestrian way or primary pedestrian route.

**Comment [ew21]:** This section needs to be restructured to include all pedestrian standards in the same location. There are repeating elements in this section and Section 235.400 Town Center Mixed Use Architectural and Pedestrian Design Standards. The recommendation is to combine all sections related to pedestrian standard, amenities and façade articulation into this section.

**Comment [ew22]:** Remove this section. All numerical standards should be relocated to a series of tables. Sidewalk widths will be determined by the Town Center Street and Path Types table or by allowed building setbacks, which is part of the development standards table.

**Comment [ew23]:** Delete. Relocate spacing standard for pedestrian connection to the Concept Plan Regulating Diagram.

**Comment [ew24]:** Relocate landscape frontage standards to the new Town Center Concept Plan Regulating Diagram.

**Comment [ew25]:** Numerical standards will be incorporated into new table "Building and Façade Standards by Street Type."

**Comment [ew26]:** Building Orientation exists under the pedestrian standards and also in 235.400 Town Center Mixed Use Architectural and Pedestrian Design Standards. Combine to this section.

- (2) On lots at major transit stops, as defined in the Transportation Planning Rule, buildings shall be located within 20 feet of the transit stop, a transit street or an intersecting street or provide a pedestrian plaza at the transit stop or street intersection.

G. **Transit Stops.** New commercial, residential, industrial or institutional uses adjoining a transit route shall provide a pedestrian connection to an existing transit stop, or to a new proposed transit stop with appropriate pedestrian amenities as required and approved by the City, in cooperation with Tri-Met.

Building and Façade Standards by Street Type Table

Design Standard	Town Center Main Street	Town Center Local Street	Universal Street	Service Street	Trail / Multi-use Path
% façade at build-to-line	60	60	60	40	NA
% landscaping at build-to-line	60	60	60	60	
Minimum ground floor height	16	14	14	14	14
Ground floor construction	1 hour fire resistive				
Separation of ground floor residential uses	Vertical distance from ground: Minimum 18 inches, Maximum 3 feet. Horizontal distance from Build to line: Minimum 3 feet / Maximum 15 feet				
Weather protection	Protected area: 20 square feet, minimum; 5 feet minimum horizontal dimension; 10 foot vertical clearance, minimum			No requirement	
Primary entry doors	Shall face street; 20% transparent, minimum	No requirement	No requirement	No requirement	
Ground floor transparency	60%	50%	50%	No requirement	

Comment [ew27]: New Building and Façade Standards Table.

Comment [ew28]: Transparency will need to be added to the Definitions in a later section but the description (visual or narrative) for how transparency will be measured will be added to this section.

**235.350 Accessory Structures.** Structures which are incidental to the primary building are considered accessory structures and are allowed when they meet the following requirements.

- A. **Covered Accessory Structures.** All covered accessory structures, whether attached to the primary building or detached, shall fulfill the front, side and rear building setbacks requirements, lot coverage and height restrictions. These standards are stated in Table 235-2.
- B. **Uncovered Accessory Structures.** Uncovered, detached accessory structures such as signs, flag poles and lamp posts are allowed in the front building setback but not in a required setback from an abutting residential zone.

**235.360 Exterior Display, Storage and Work Activities.**

- A. **Exterior Display.** Exterior display of goods is allowed for commercial uses only. The setback and landscape standards for exterior display are stated in Table 235-3.
- B. **Exterior Storage.** Exterior storage is prohibited.
- C. **Exterior Work Activities.** Exterior work activities are prohibited except for restaurants, plant nurseries, entertainment and recreation uses and outdoor markets.
- D. **Paving.** All exterior display and work areas, except plant nurseries, shall be paved.

<b>TABLE 235-3</b> Exterior Display Setbacks and Landscaping in Town Center Zone (See Section 235.360)	
Commercial Use Category	Standards
Exterior Display	
- Abutting a street	5 ft/L1
- Abutting Commercial Use Category	None
- Abutting Other Use Category	5 ft/L1
- Abutting any R zone	5 ft/L3

Note: For description of the landscape requirements (i.e. L3, F2, etc.), see Section 330 - Landscaping & Screening.

**235.370 Open Area Requirement.**

- A. **Purpose.** The open area requirement ensures provision of adequate amounts of light and air, and facilitates circulation for pedestrians throughout areas in the Town Center zone. These requirements create open areas and pedestrian connections.
- B. **Required Amount of Open Space.** On lots larger than 250,000 square feet, at least 30 percent of the area that exceeds 250,000 square feet must be devoted to open area.
- C. **Standards.**
  - (1) Open areas include: public and private streets; parks; plazas; covered or uncovered walkways; public fountains; and landscaped features or areas, including required landscaping. Open areas do not include areas used for parking lots; motor vehicle loading, maneuvering and delivery. When private streets are proposed to meet the open area requirement, at least one side of the street must be provided with sidewalks and street trees. Fifty percent of the area of a street's right-of-way, for the length of the right-of-way abutting a lot, may be used to meet that lot's open area requirement.
  - (2) The open area must include, at a minimum, paved walkways connecting development on the lot to each lot line that borders a public or private street.
- D. **Application of the Requirement.** The open space requirement is triggered by construction of any new floor area for any use. The required open space may be provided on the same site or on another site that is zoned Town Center and that is subject to the same Master Plan approval under the provisions of Section 235.500.
- E. There is an estimated 2.5 acres wetland in the northeast corner of the Town Center zone that adjoins Wood Village Blvd. and Arata Road. This area shall be utilized as the required open space at the time of site re-development. Open space landscape plans shall be primarily passive improvements that protect the resource and enhance water quality while providing connectivity and natural open space in the area.

**Comment [ew29]:** Consider addressing issues about how this open space provision functions, and the relative equity for larger parcels. Recommend modifying this language to make the requirement either the same, or similar for properties of all sizes.

**235.371 Town Center Gateways**

**Comment [ew30]:** Consider how the open space requirement results in useable space (such as dedicated parks) for the Town Center.

**235.375 Transportation System Standards**

- A. **Regional Transportation Plan Standards.** After Metro adoption of Regional Transportation Plan amendments, applications for design review (or for land division for developments not requiring design review) must demonstrate compliance with any applicable requirements of the Regional Transportation System Plan or any applicable Transportation System Plan. Demonstration of compliance must include a showing that adopted level of service standards are met for each proposed phase of development.

**Comment [ew31]:** New section: This section will include development standards for gateways (as indicated on the new Concept Plan Regulating Diagram). The development standards will address the role of gateways in providing for transit, public space, pedestrian orientation and signage. Gateway language will be primarily focused around building and landscape frontage treatments.

- B. **Conditions.** The City may impose conditions on development necessary to achieve LOS standards, including the construction of offsite improvements; but no improvements to streets or intersections not contiguous to the subject property shall be undertaken without the approval of the affected neighboring city.

**235.380 Fences.**

- A. **Types of Fences.** The fencing standards apply to walls, fences and screens of all types whether open, solid, wood, metal, wire, masonry or other material including vegetative materials.
- B. **Location and Height Limits.**
  - (1) Fences up to 3 ½ feet are allowed in required front building setback and required side building setbacks to the depth of the required front building setback.
  - (2) Fences up to 8 feet are allowed in required side and rear building setbacks except for corner lot side setback which is limited to 3 ½ feet.
  - (3) The location of fences shall not interfere with the pedestrian circulation system.

**235.390 Additional Regulations.**

- A. **Demolitions.** Demolition of all buildings is regulated by the Building Code currently adopted by the City of Wood Village.
- B. **Design Review.** Design Review is required for all uses in this mixed use zone. See Section 630. In addition to the Design Review requirements of Section 630, the Design Review Board shall review all specific development applications within a Master Planned area to ensure that the site designs and building locations encourage safe and convenient pedestrian and bicycle access; are reasonably accessible to transit; and provide adequate pedestrian connections to exterior streets and to other uses within the Master Planned area through clustering of buildings, construction of accessways and walkways or other similar techniques.
- C. **Parking and Loading.**
  - (1) The requirements of Section 350 apply.
  - (2) In addition, parking requirements in the Town Center zone may be met by blending parking rates. Calculating parking stalls for a use may include counting adjacent on-street spaces, nearby public parking, cross-patronage, and shared parking possibilities due to variation in hours of operation and as per Section 350.045(5).

- (3) In addition to the requirements of Section 350, for all institutional, office and industrial uses having more than 20 auto parking spaces on the site, the following standards must be met:
  - (a) Five spaces or five percent of the parking spaces on the site for such institutional, office and industrial uses, whichever is less, must be reserved for carpool use before 9:00 AM on weekdays.
  - (b) The spaces will be those closest to the building entrance or elevator, but not closer than the spaces for disabled parking and those signed for exclusive customer use.
  - (c) Signs must be posted indicating these spaces are reserved for carpool use before 9:00 AM on weekdays.
- (4) A parking area other than on-street parking shall not be located between a street and a main building entrance described in Section 235.345.F(1). A street between a main building entrance described in Section 235.345.F(1) and a parking area may be a private street, as long as it satisfies the applicable pedestrian standards of Section 235.345.
- (5) In lieu of the provisions of Section 350.075.C and Table 350-4, loading areas shall be set back five feet from a street lot line, or a segment of the pedestrian circulation system required by Section 235.345, and landscaped to the L3 standard.
- D. **Public Access.** All lots shall have frontage or approved access to public streets, public water and public sewer before development is allowed. Vehicular access to commercial uses shall be from the highest classified or capacity street.
- E. **Sale or Conveyance Prohibited.** No sale or conveyance of any portion of a lot, for other than a public purpose, shall leave a structure on the remainder of the lot with less than the minimum setback requirements of the zone.
- F. **Signs.** Signs in the Town Center shall be approved and constructed in accordance with Section 370 Signs, except that freestanding signs in the Town Center Zone must be either a Wood Village Town Center sign, or a center-wide directory-type freestanding sign. Individual building pads may not erect a freestanding sign that advertises only their business. See Section 370.
- G. **Solid Waste, Mixed and Recyclables Storage.** See Section 390.
- H. **Uncontained hazardous materials are prohibited.**
- I. **Street Standards.** In order to connect residential areas with services and institutions by short and direct public accessways, development plans must include a sufficient number of collector and other streets to provide one street every 530 feet, unless

**Comment [ew32]:** Delete.

**Comment [ew33]:** Move this sub- section 235.390 (I) into section 235.345.

**Comment [ew34]:** Remove this section. The Concept Plan Regulating Diagram and accompanying table will determine street/path locations and types and their required intersections.

impractical due to existing development, topography, physical barriers, or environmental constraints. This street must provide a through or planned connection to another street that connects to the existing street system. Where possible or planned in accordance with the City, County or Regional Transportation Plans, extend new streets to existing streets. Further, cul-de-sacs may not exceed 200 feet in length and provide access to more than 25 dwelling units, except for the long, deep parcels adjoining NE Glisan Street where cul-de-sacs may be constructed up to 400 feet.

- J. **Bicycle Path Connections.** Development plans should include bicycle path connections every 330 feet within the development and to adjoining properties unless impractical due to existing development, topography, physical barriers or environmental constraints.

**Comment [ew35]:** Remove this section. The Concept Plan Regulating Diagram and accompanying table will determine street/path locations and types and their required intersections.

**235.400 Town Center Mixed Use Architectural and Pedestrian Design Standards**

- A. **Purpose and Applicability.** This section is intended to provide detailed, human-scale design that is characteristic of Wood Village, while affording flexibility to use a variety of architectural building styles. All new buildings and major remodels shall meet the standards of these subsections which are applied through Site Design Review. The applicant demonstrates that the standards are met by complying with the criteria under each standard.

**Comment [ew36]:** Combine all of this section into the Pedestrian Standards (section 235.345). Revise language of this section to include clear and objective standards about building form, building frontages and additional intersection requirements.

- B. **Architectural Standards:**

1. **Building Mass:** Where building elevations are oriented to the street or other public places, architectural features such as articulated facades, building off-sets, varied roof lines, pitched roofs and gables, windows, and pedestrian entrances shall be used to avoid large monotonous building surfaces and large building masses.
2. **New buildings:** Should architecturally transition in terms of bulk and mass to respect adjacent existing buildings. This transition can be accomplished by selecting designs that “step down” or “step up” from small to big structures and vice versa. Transitions can also take the form of carrying building patterns and lines (e.g., parapets, windows, etc.) from the existing building to the new one.
3. **Building Block Layout:** The site shall be configured into blocks that have frontage onto streets, interior parking courts or common open space. Blocks shall not exceed 400 feet in length.
4. **Building Orientation:** Developments subject to this Section shall have their buildings oriented to a public or private street, as generally shown in the Figure below. This standard is met when all of the following criteria are met:
  - a. Compliance with the setback standards in Table 235-2 where applicable. The front yard setback may be a maximum of 10’ to provide pedestrian amenities between a building and its adjoining street;

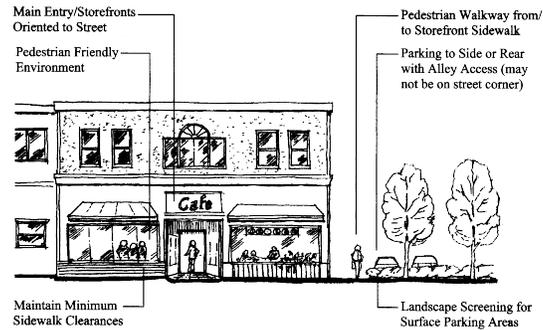
**Comment [ew37]:** Revise section to include clear and objective standards about minimum floor to ceiling height, minimum percentage of ground floor transparency and ground floor transparency at intersections. Move these standards to new table (in pedestrian standards section) Building and Façade Standards by Street Type.

**Comment [ew38]:** Remove section. Concept Plan Regulating Diagram will specify building block layout in the Town Center. Based on the CP Regulating Diagram, block lengths are varied depending on the locations of the required intersections. These intersections would have some flexibility in location (up to 50 feet in either direction).

**Comment [ew39]:** Primary building opening would be permissible on side locations, provided that the entry is visible from the main street.

- b. Except as provided in subsections below, all buildings shall have at least one primary building entrance (i.e., dwelling entrance, a tenant entrance, lobby entrance, or breezeway/courtyard entrance) facing an adjoining street (i.e., within 45 degrees of the street property line), or if the building entrance is turned more than 45 degrees from the street (i.e., front door is on a side elevation), the primary entrance shall not be more than 20-40 feet from a street sidewalk, except to provide pedestrian amenities; a walkway shall connect the primary entrance to the sidewalk in this case.

**Figure 1 – Building Orientation**



- c. In a mixed-use development, off-street parking, driveways, and other vehicle areas shall not be placed between buildings and the street(s) to which they are oriented; Off-street parking in a mixed-use development shall be oriented internally to the site and divided by landscape areas into bays of not more than [24] parking spaces per bay.
5. **Building Materials:** Natural materials such as wood, veneer siding, stucco, masonry, or glass shall be the primary exterior structural siding materials. Also use reclaimed or new brick, cultured stone, or other rapidly renewable resources. Unfinished metal siding, corrugated sheet metal, vinyl siding, T1-11 siding or plywood, and reflective glass are prohibited. Whenever, possible, use energy efficient building materials in accordance with LEED Certification standards.
- a. **Colors:** Generally subdued earth tones in gray, brown, off-white, slate, green and black. Accent trims and other colors may also used as approved by the Design Review Board.
6. **Roof-mounted Equipment:** All roof-mounted equipment, including satellite dishes and other communication equipment, must be screened in one of the following ways. Solar heating panels are exempt from this standard:
- A parapet as tall as the tallest part of the equipment;
  - A screen around the equipment that is as tall as the tallest part of the equipment;

- c. The equipment is set back from the street facing perimeters of the building four feet for each foot of height of the equipment; or,
  - d. If the equipment is a satellite dish or other communication device, it is added to the façade of a penthouse that contains mechanical equipment, is no higher than the top of the penthouse, is flush mounted, and is painted to match the façade of the penthouse.
7. **Roofs:** Buildings must have either a sloped roof with a pitch no flatter than 4/12 or a flat roof with a cornice that meets the following:
- a. There must be two parts to the cornice. The top part of the cornice must project at least 6 inches from the face of the building and be at least 2 inches further from the face of the building than the bottom part of the cornice; and
  - b. The height of the cornice is based on the height of the building as follows:
    - Buildings 18 feet or less in height must have a cornice at least 12 inches high.
    - Buildings greater than 18 feet and less than 30 feet in height must have a cornice at least 18 inches high.
    - Buildings 30 feet or greater in height must have a cornice at least 24 inches high.
8. **Pedestrian-Orientation:** The design of all buildings on a site shall support a safe and attractive pedestrian environment. This standard is met when the approval body finds that all of the criteria in 1-7, below, are met. Alternatively, the approval body may approve a different design upon finding that the design contains an equally good or superior way of achieving the above standard.
- a. The building orientation standards under Section 235.400 B(4) are met; and
  - b. Primary building entrances shall open directly to the outside and, if not abutting a street, shall have walkways connecting them to the street sidewalk; every building shall have at least one primary entrance that does not require passage through a parking lot or garage to gain access; and
  - c. Corner buildings (*i.e.*, buildings within 20 feet of a corner as defined by the intersecting curbs) shall have corner entrances, or shall provide at least one entrance within [20] feet of the street corner or corner plaza; and
  - d. At least [20-60] percent of the building's front façade (measured horizontally in linear feet) shall be located at the build-to line or closer to the street, unless modified by the Design Review board; and

**Comment [ew40]:** All numerical standards of this section will be relocated to the new table Building and Façade Design by Street Type.

**Comment [ew41]:** Change percentage of building frontage required to meet the edge of the sidewalk or the edge of the allowable setback to 60%. A setback range (described in Development Standards in Town Center Zone table) will allow for flexibility of building edge and provide opportunities for pedestrian amenities. Specific development standards for ground plane treatments within setbacks on development sites will be provided.

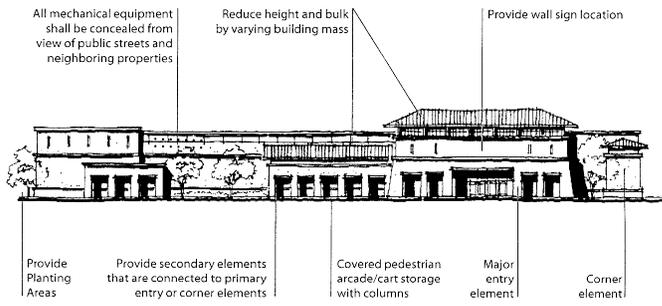
- e. Ground floor windows or window displays shall be provided along at least [30-60] percent of the building's (ground floor) street-facing elevation(s); windows and display boxes shall be integral to the building design and not mounted to an exterior wall; and
  - f. Primary building entrance(s) are designed with weather protection, such as awnings, canopies, overhangs, or similar features; and
  - g. Drive-up and drive-through facilities are not allowed except on Lots 1 through 17, Wood Village Town Center, Wood Village, OR. (Ord. 15-2010)
9. **Compatibility.** All new buildings and major remodels shall be designed consistent with the architectural context in which they are located. This standard is met when the approval body finds that all of the criteria in 1-7, below, are met.
- a. There is continuity in building sizes between new and existing buildings;
  - b. The ground floor and upper floor elevations and architectural detailing are compatible with adjacent commercial buildings;
  - c. Buildings adjacent to the TC Zone height step-down, as required by Section Building Height Transition.
  - d. Roof elevation is compatible with adjacent commercial buildings (roof pitch, shape, height step-down);
  - e. There is continuity of building sizes on the site if more than one building is proposed;
  - f. There is continuity in the rhythm of windows and doors on the proposed building(s);
  - g. The relationship of buildings to public spaces, such as streets, plazas, other areas, and public parking, including on-street parking, is strengthened by the proposed building(s).
10. **Human Scale.** The design of all buildings shall be to a human-scale. This standard is met when the approval body finds that all of the criteria in 1-6, below, are met. Alternatively, the approval body may approve a different design upon finding that the design contains an equally good or superior way of achieving the above standard. Figure 1 contrasts examples of building elevations that are consistent/inconsistent with human scale criteria.
- a. Regularly spaced and similarly-shaped windows are provided on all building stories;

**Comment [ew42]:** "Compatibility" may no longer be appropriate for the Town Center code.

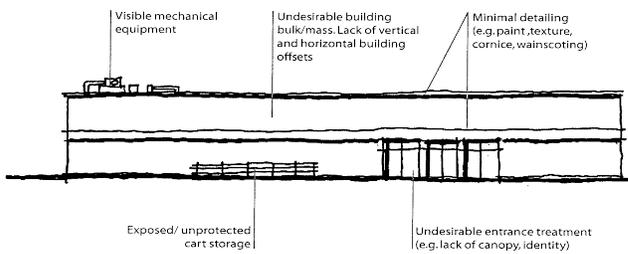
- b. Ground floor retail spaces have tall ceilings (*i.e.*, 12-16 feet) with display windows on the ground-floor;
- c. Display windows are trimmed, recessed, or otherwise defined by wainscoting, sills, water tables, or similar architectural features;
- d. On multi-story buildings, ground floors are defined and separated from upper stories by appropriate architectural features (*e.g.*, cornices, trim, awnings, canopies, arbors, trellises, overhangs, or other features) that visually identifies the transition from ground floor to upper story; such features should be compatible with the surrounding architecture;
- e. The tops of flat roofs are treated with appropriate detailing (*i.e.*, cornice, pediment, flashing, trim, or other detailing) that is compatible with the surrounding architecture;
  - 1. Pitched roofs have eaves, brackets, gables with decorative vents, or other detailing that is consistent with the surrounding architecture;
  - 2. Historic design and compatibility requirements, where applicable, are met; and
- f. Where buildings with greater than [8,000] square feet of enclosed ground-floor space are proposed, they shall provide articulated facades on all street-facing elevations. This criterion is met when an elevation contains at least one of the following features for every [40] feet of building (horizontal length): windows; primary entrances; weather protection (awnings, canopies, arbors, trellises), building offsets; projections; changes in elevation or horizontal direction; sheltering roofs; terraces; a distinct pattern of divisions in surface materials; ornamentation; screening trees; small-scale lighting (*e.g.*, wall-mounted lighting, or up-lighting); and/or similar features as generally shown in Figure 2.3.170.D. *[Note: Figure 2.3.170.D should not be interpreted as a required architectural style.]*

**Comment [ew43]:** Move minimum ground floor height to Building and Façade Table by Street Type Table with 16-foot minimum for the main street and lower height minimums for other street types.

**Figure 2 – Examples of Large Commercial Design Elements**



**Large Commercial Massing - Acceptable**



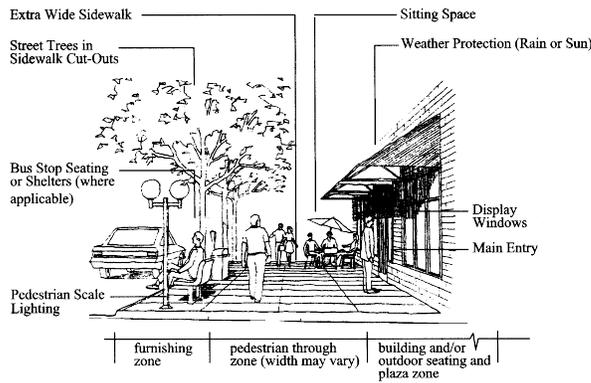
**Large Commercial Massing - Unacceptable**

C. **Pedestrian and Transit Amenities**

**Comment [ew44]:** Incorporate into Pedestrian Standards section. Some of the elements of this section will be regulated by street type, such as sidewalk widths and entrance canopy (weather protection) standards.

1. **Purpose and Applicability.** This section provides standards for pedestrian amenities when pedestrian amenities are required as part of new developments or major remodels in the Town Center Zone and when pedestrian amenities are provided to meet the requirements of other code sections. Pedestrian amenities serve as informal gathering places for socializing, resting, and enjoyment along street frontages and contribute to a walkable district.
2. **Standards.** New developments and major remodels in the Town Center Zone and other developments subject to the provisions of this section shall provide *one* or more of the “pedestrian amenities” listed below, and as generally illustrated in Figure 3. Pedestrian amenities may be provided within a street furnishing zone, building frontage zone, or plaza, or within the pedestrian through zone. Use of the public right-of-way requires approval by the roadway authority.
  - a. A plaza, courtyard, square or extra-wide sidewalk next to the building entrance (minimum width of [6] feet);
  - b. Sitting space (*i.e.*, dining area, benches, garden wall or ledges between the building entrance and sidewalk) with a minimum of 16 inches in height and 30 inches in width;
  - c. Building canopy, awning, pergola, or similar weather protection (minimum projection of 4 feet over a sidewalk or other pedestrian space);
  - d. Public art that incorporates seating (*e.g.*, fountain, sculpture).
  - e. Transit amenity, such as bus shelter, per the standards of Tri-Met.

**Figure 3 – Examples of Pedestrian [and Transit] Amenities**



**235.500 Master Plan**

A. **Purpose.** A Master Plan is required for all sites in the Town Center zone to ensure that transportation and other facilities will support the anticipated development, and comply with requirements of the zone; and to identify the location within which housing requirements will be met. The Master Plan provides an opportunity to ensure that Comprehensive Plan policies concerning home ownership and local street systems are satisfied in new developments in the Town Center zone.

**Comment [ew45]:** Relocate the Master Plan section to the beginning of the entire Town Center Zone section 235.

B. **When Required.** The Planning Commission and City Council shall review and approve a Master Plan meeting the requirements of this section before any land division or other development may occur on a site in the Town Center Zone.

**Comment [ew46]:** A Master Plan is required for all sites in the Town Center zone to ensure that transportation and other facilities will support the anticipated development, and comply with requirements of the zone. The City shall adopt, for the entire Town Center Zone, a Concept Plan Regulating Diagram using a Type III procedure. The adopted Concept Plan Regulating Diagram will provide the framework for the review of applicant submitted master plans for portions of the site.

C. **Procedures.**

(1) **Pre-Application Review of Development Plan.**

(a) Prior to submitting an application for a Master Plan, the applicant shall submit a conceptual development plan to City staff for review. Such plan shall consist of a general site plan and brief narrative containing the following elements:

**Comment [ew47]:** Change to "proposed"

- (i) Proposed land uses and estimated densities,
- (ii) Street and parking circulation system,
- (iii) Water transmission lines, and
- (iv) Major storm and sanitary sewer facilities.

(b) City Staff shall review the conceptual plan and prepare a pre-application development plan report responding to the conceptual plan within 30 days of submittal. The report shall be based upon the Comprehensive Plan, the standards of this ordinance and regulations on the suitability of the proposed development in relation to the character of the area.

**Comment [ew48]:** Add section (v) Conformity with the adopted Concept Plan Regulating Diagram for the site.

(2) **Master Plan.**

(a) The applicant must submit a Master Plan application within one year of pre-application development plan review by the Planning Commission. Otherwise, the pre-application review will expire and another pre-application review will be required prior to submission of a Master Plan.

(b) The Master Plan shall be reviewed under the Type III procedures listed in Section 520 of the Code. The Planning Commission's decision to approve, approve with conditions or deny the Master Plan shall be based on the requirements of Section 235.500.D and shall be in the form of a recommendation to the City Council. The City Council must review the Planning Commission's decision on the record and determine whether the Master Plan should be approved, approved with conditions or denied.

(3) **Changes and Modifications.**

- (a) **Major Changes.** Major changes in the Master Plan after it has been adopted shall be considered the same as a new application and shall be made in accordance with the procedures specified in this section.
- (b) **Minor Changes.**
  - (i) Minor changes in the Master Plan may be approved by the Planning Commission, provided that such changes:
    - 1. Do not change densities;
    - 2. Do not change boundaries; and
    - 3. Do not change the location or amount of land devoted to specific land uses.
  - (ii) Such changes may include minor shifting of the location and size of buildings, proposed streets, public or private ways, utility easements, parks or other public spaces, or other features of the plan.

**D. Required Contents.**

- (1) **Master Plan Diagram.** The Master Plan shall address the entire site.
  - (a) In addition to the requirements of Section 500.110, a Master Plan Diagram must include the following:
    - (i) A vicinity map, including zoning and uses of adjacent sites;
    - (ii) Scale (at least one inch equals 50 feet or larger);
    - (iii) North arrow;
    - (iv) Date;
    - (v) Approximate location and size of planned land uses including areas not covered by buildings;
    - (vi) Site topography at a contour interval not to exceed 5 feet; and
    - (vii) Locations of all natural features including, but not limited to, any existing trees of a caliper greater than 6 inches dbh, and any natural drainage ways, wetlands or creeks on the site and identification of any contemplated modifications of natural features.

(b) The Master Plan shall include a diagram showing the approximate location of public facilities and services to support planned land uses. The public facilities shown shall include:

- (i) Names and locations of any proposed collector streets, and, if their general locations can be reasonably anticipated, other streets. Except where topographical constraints or arterial street access standards preclude it, the diagram shall include a sufficient number of collector and other public or private streets to provide one street every 660 feet;
- (ii) Pedestrian pathways, if their general locations can be reasonably anticipated;
- (iii) Transit stops, if their general locations can be reasonably anticipated;
- (iv) Major storm and sanitary sewer facilities;
- (v) Major water transmission lines;
- (vi) Public facilities, including streets, sewer, water and storm sewer on adjacent properties;
- (vii) Street and pedestrian connections, if any, to existing streets or paths on adjacent properties; and
- (viii) Any other expected public facilities if their general locations can be reasonably anticipated.

**Comment [ew49]:** Clarify to include the full range of Street and path types (Main, Local, Universal, Service, Trail / Multi-use path) shown on Concept Plan Regulating Diagram. Location and type of streets and paths needs to be consistent with CP Regulating Diagram.

**Comment [ew50]:** There are conflicting provisions in this code for block lengths. The recommendation is to use the Concept Plan Regulating Diagram (new graphic) for the Town Center block length and retain 530-foot block length standards elsewhere in the system. In addition, consideration should be given to adding a spacing standard for pedestrian through connection, such as 350 feet.

**Comment [ew51]:** Including open space requirements

(b) The Plan diagram need not include the specific location of particular developments, buildings or lots. The diagram shall indicate the general expected location of residential uses, commercial and limited industrial uses and any areas of mixed use, any existing buildings to remain and the specific location of land set aside for required housing under Section 235.250 or smaller retail uses under Section 235.275. The Plan diagram shall specify the location of the focal or community feature required by Section 235.285.

**Comment [ew52]:** The diagram should contain building footprints, or at least maximum building footprints, where entitlements could be granted up to the footprint. If a specific building does not match, the minor amendment provision of the code would apply. It should be generally understood which streets, paths and open spaces future buildings will be oriented to.

**Comment [ew53]:** Delete this language.

(2) **Master Plan Text.** The Master Plan shall include a narrative explaining how the following requirements are met:

**Comment [ew54]:** Much of this detail in the submittal requirements would be better implemented using a diagram rather than a narrative format.

- (a) The Transportation Policies of the Comprehensive Plan.
- (b) Comprehensive Plan policies regarding home ownership and mix of types of housing and the requirements of Section 235.250.
- (c) Where applicable, the schedule for future construction of required housing as provided in Section 235.250.B(2).

**Comment [ew55]:** Delete subsection (b) and (c)

- (d) The Community Retail requirements of Section 235.275.
- (e) The Open Area requirement of Section 235.370.
- (f) The Non-Retail Employment Use requirement of 235.290 or 235.295, whichever applies.
- (g) The Community Feature requirement of 235.285.
- (h) How the proposed development responds to the "Vision of Wood Village in 2020."
- (i) Any other requirements of this Code that the Planning Commission or City Council determine must be addressed in the Master Plan in order to ensure conformance with those requirements.

**E. Effect of Plan.** A Master Plan is not a subdivision, planned unit development or specific design review approval for land development. An applicant must obtain separate approvals for any development, including design review, subdivision or planned unit development approvals. A project or land division that is generally consistent with the Master Plan may proceed under the regular provisions of this development code. If a proposal would, in the judgment of the Planning Commission or Design Review Commission, be clearly inconsistent with the general designations in the Master Plan, the applicant must seek amendment of the Master Plan either before or concurrently with the application for design review, subdivision or planned unit development approval.

**F. Variances**

**Comment [ew56]:** The adopted Concept Plan Regulating Diagram will provide the framework for the review of applicant submitted master plans. The new section will define specific allowable variances from the Concept Plan Regulating Diagram. Selected features of the CP Regulating Diagram will be identified as not eligible for variance. Variance procedure is listed in section 660 of this zoning code.

# Concept Plan Regulating Diagram

CITY OF WOOD VILLAGE, OREGON, TOWN CENTER PLAN | DRAFT CONCEPT PLAN REGULATING DIAGRAM



**Comment [ew57]:** New Graphic: Concept Plan Regulating Diagram showing street and path type locations, required intersections and building and landscape frontage requirements. Required intersections would have some flexibility in location (up to 50 feet in either direction).

Draft Concept Plan Regulating Diagram

12 OCTOBER 2016 | WP | PARSONS BRINCKERHOFF | URBWORKS, INC | LELAND CONSULTING GROUP | COGAN OWENS GREENE

**Other sections of the zoning code that require amendments:**

**SECTION 330 LANDSCAPING AND SCREENING**

**SECTION 630 DESIGN REVIEW**

**630.070 Design Review Plan Contents.**

- A. Any preliminary or final design review plan shall be filed on forms provided by the City Administrator and shall be accompanied by such drawings, sketches and descriptions as are necessary to describe the proposed development. A plan shall not be deemed complete unless all information requested is provided.
  
- B. Contents:
  - (1) Preliminary Site Development Plan;
  - (2) Preliminary Site Analysis Diagram;
  - (3) Preliminary Architectural Drawings, indicating floor plans and elevations;
  - (4) Preliminary Landscape Plan;
  - (5) Design Review Application Fee;
  - (6) For developments that generate more than 400 average daily motor vehicle trips (ADT's), the applicant shall provide adequate information, such as a traffic impact study or traffic counts, to demonstrate the level of impact to the surrounding street system.

**Comment [ew1]:** Add provision in this section for additional types of acceptable landscape edges such as a low wall and trellis. Gateway landscaping may also appear in this section.

**SECTION 710 USE CATEGORIES**

**SECTION 720 DEFINITIONS**

**Comment [ew2]:** Need more specific language to specify that an existing and future (20 year) analysis is required that documents not only the existing and future v/c ratios, but also the multi-modal safety impacts of the additional traffic (e.g. the potential for future backups at interstate off-ramps that could lead to safety concerns).

**Comment [ew3]:** Add lodging to use category

**Comment [ew4]:** Add Lodging, Mixed-use, and Transparency to the definitions section.

## 60.55.15.1.

- C. The volume of trips that the development will add on the residential street during each of those same hours.
  - D. Recommended traffic management strategies designed to City standards to mitigate the impacts of the increased trips attributed to the development. Potential traffic management strategies include, but are not limited to, any combination of speed humps, curb extensions, intersection treatments, and traffic control devices.
2. The Traffic Management Plan shall discuss whether the recommended improvements both on-site and off-site are justified, reasonably related to, and roughly proportional to the impacts of the proposed development and shall include information sufficient for the City to assess whether the proposed mitigation strategies are reasonably related and roughly proportional to the level of impact. [ORD 4103; May 2000]

**60.55.20. Traffic Impact Analysis.** [ORD 4103; May 2000] [ORD 4302; June 2004] For each development proposal that exceeds the Analysis Threshold of 60.55.20.2, the application for land use or design review approval shall include a Traffic Impact Analysis as required by this code. The Traffic Impact Analysis shall be based on the type and intensity of the proposed land use change or development and its estimated level of impact to the existing and future local and regional transportation systems.

- 1. **Engineer Certification.** The Traffic Impact Analysis shall be prepared and certified by a traffic engineer or civil engineer licensed in the State of Oregon.
- 2. **Analysis Threshold.**
  - A. A Traffic Impact Analysis is required when the proposed land use change or development will generate 200 vehicles or more per day (vpd) in average weekday trips as determined by the City Engineer.

60.55.20.2.

- B. A Traffic Impact Analysis or some elements of a Traffic Impact Analysis may be required when the volume threshold under subsection A. of this section is not met but the City Engineer finds that the traffic impacts attributable to the development have the potential to significantly impact the safe and efficient operation of the existing public transportation system.
3. **Study Area.** The Traffic Impact Analysis shall evaluate the Area of Influence of the proposed development and all segments of the surrounding transportation system where users are likely to experience a change in the quality of traffic flow. The City Engineer may identify additional locations for study if existing traffic operation, safety, or performance is marginal or substandard. Prior to report preparation, the applicant shall submit the proposed scope and analysis assumptions of the Traffic Impact Analysis. The City Engineer shall determine whether the scope and analysis assumptions are adequate.
  4. **Contents of the Traffic Impact Analysis Report.** The Traffic Impact Analysis report shall contain the following information organized in a logical format:
    - A. Executive Summary
    - B. Description of Proposed Development
    - C. Existing Conditions
    - D. Traffic Forecasts
    - E. Traffic Impacts
    - F. Mitigation Identification
    - G. Recommendations
    - A. Executive Summary. An Executive Summary of no more than three single-sided pages shall be included at the beginning of the Traffic Impact Analysis report. The Executive Summary shall summarize the analysis and conclusions and identify recommended transportation improvements.

60.55.20.4.

- B. Description of Proposed Development. The Traffic Impact Analysis shall provide a comprehensive project description including but not limited to the following:
1. Vicinity map.
  2. Site plan.
  3. Project phasing.
  4. Time schedule.
  5. Intended use of the site, including the range of uses allowed without additional land-use approvals.
  6. Intensity of use.
- C. Existing Conditions. The Traffic Impact Analysis shall provide a complete evaluation of existing conditions and include maps and/or tables displaying the following information for the Area of Influence and any additional locations previously identified by the City Engineer:
1. Street system including street names and functional classifications.
  2. Pavement and shoulder widths.
  3. Striping and channelization.
  4. Driveways.
  5. Freight access and loading areas.
  6. Intersections.
  7. Traffic volumes.
    - a. Existing traffic shall be measured within the previous twelve months.
    - b. Traffic volumes shall be based on data from a minimum of three typical weekdays. In addition, data shall be provided for weekends if weekends are the peak traffic period for either the existing street or the proposed development.
    - c. Seasonal variations in traffic volumes shall be considered.
  8. Existing intersection performance indicators including volume-to-capacity ratio and control delay.
  9. Transit information including stop and shelter locations, route numbers, headways, passenger loading, pull outs, and times of service.
  10. Bicycle ways, sidewalks, and accessways.
  11. Collision data for the most recent three-year period for which collision data is available.

60.55.20.4.

- D. Traffic Forecasts. The Traffic Impact Analysis report shall provide forecasts of future traffic within the Area of Influence and any additional locations previously identified by the City Engineer. Traffic forecasts shall be provided for both the Buildout Year and the Long-Range Forecast Year. The report shall include complete documentation of trip generation calculations including Institute of Transportation Engineers (ITE) Trip Generation (latest published edition) use code(s) or an alternative basis of trip generation and the rationale for using the alternative.
1. Buildout Year Analysis. Buildout Year forecasts shall be Total Traffic at the time of anticipated completion and occupancy of each phase of the development and at the time of completion and occupancy of the entire development. The City shall provide traffic information on other developments to consider in the calculation of Added Traffic.
  2. Long-Range Forecast Year Analysis. The Traffic Impact Analysis shall include an analysis of the potential worst-case long-range impacts to the local transportation system identified in the City's Comprehensive Plan Transportation Element and the regional transportation system identified in Metro's Regional Transportation Plan. The forecast year shall be the forecast year of the Comprehensive Plan Transportation Element or an alternate year approved by the City Engineer. The Traffic Impact Analysis shall include a prediction of whether any phase of the proposed development will change the long-range transportation needs identified in the Comprehensive Plan and the extent to which traffic from the proposed development contributes to the long-range improvement needs.

60.55.20.4.D.

3. Traffic Forecast Analysis Assumptions.

- a. Trip generation. Estimates of the proposed development's trip generation shall be made for peak period traffic. Selection of the peak period used in the analysis shall be justified and shall consider, at a minimum, the peak period for the proposed development and the peak period for surrounding streets. The City Engineer may require review of other time periods based on known or anticipated marginal or substandard traffic capacity or traffic safety. Trip generation estimates shall be based on ITE's Trip Generation (latest published edition). The City Engineer may approve different trip generation rates when trip generation rates are not available in ITE's Trip Generation or different rates are justified.
- b. Trip distribution and assignment. Traffic generated by the proposed development shall be logically distributed and assigned to the street system within the Area of Influence and any additional locations previously identified by the City Engineer. Trip distribution and assignment shall be based on trip distribution information from Washington County, ODOT, or Metro, on analysis of local traffic patterns based on data less than 12 months old, or on alternative data approved by the City Engineer.

4. Intersection and Highway Interchange Analysis.

Intersection and highway interchange analysis shall conform to the method for operations analysis described in the Highway Capacity Manual 2000 published by the Transportation Research Board. The City Engineer may approve an alternative analysis method. The analysis shall document that the impacts of queuing from adjacent intersections or traffic restrictions has been addressed.

60.55.20.4.

- E. Traffic Impacts. The Traffic Impact Analysis shall evaluate access, safety, operation, capacity, circulation, level of service, and performance of the transportation system within the proposed development's Area of Influence and any additional locations previously identified by the City Engineer for both the Buildout Year and any phases thereof, and the Long-Range Forecast Year.

Performance analysis shall be based on the standards of Section 60.55.10.7.

1. Safety considerations shall be evaluated. Potential safety problems resulting from conflicting turning movements between and among driveways, intersections, and internal traffic shall be addressed. Distance to the nearest driveways on both sides of streets fronting the site and in both directions from site access points shall be shown. On-Site driveway stacking and queuing impacts shall be assessed. The potential for shared access with adjacent development shall be assessed.
2. Geometric design and operational improvements including but not limited to acceleration lanes, deceleration lanes, turning lanes, traffic signals, and channelization shall be considered, evaluated, and recommended when determined necessary by standards and practices adopted by ODOT, Washington County, the City or approved by the City Engineer.
3. Adequacy of sight distance shall be addressed at the proposed road access point(s) for both the existing road configuration and for the ultimate road configuration based on improvements planned for the development and improvements identified in the Comprehensive Plan Transportation Element. Sight distance shall meet City standards.
4. The analysis shall also identify and evaluate related impacts on bicycle, pedestrian, and transit access, circulation, and facilities.

## 60.55.20.4.E.

5. Other, operational, circulation, safety, and capacity issues shall be evaluated and addressed as required by this code and by the City Engineer.

- F. Mitigation Identification. In order to protect the public transportation system from potentially adverse impacts of the proposal, to fulfill an identified need for public services within the impacted area related to the development, or both, the Traffic Impact Analysis shall identify methods of mitigating on-site and off-site deficiencies for present and proposed phases of the development. The analysis shall make recommendations for improvements necessary for safe and efficient traffic flow and bicycle, pedestrian, and transit movement and access based on and roughly proportional to the identified impacts. Buildout Year, Long-Range Forecast Year, and project phasing impacts shall be considered. [ORD 4418, February 2007]

The traffic impact analysis shall discuss the estimated levels of impact, improvements, and mitigations, and shall demonstrate how the recommended mitigations are roughly proportional to the identified impacts. [ORD 4418, February 2007]

Mitigation shall be consistent with improvements identified in the Comprehensive Plan Transportation Element. At a minimum, the Traffic Impact Analysis shall consider ultimate rights-of-way and additional streets, bicycle, and pedestrian connections and extensions and intersection improvements that are identified in the Comprehensive Plan Transportation Element Figures 6.1 through 6.23 and Tables 6.1 through 6.6 and connections required by Section 60.55.25. of this code. Mitigation measures may also include, but are not limited to, additional street connections and street extensions, turn lanes, signalization, signal modifications, installation of medians, shared access and other access management strategies, geometric improvements such as lane geometry improvements, and intersection realignments.

Where stop-controlled intersections do not meet the minimum performance standard of Section 60.55.10.7., an additional street connection or a street extension shall be considered as a potential mitigation measure.

## 60.55.20.4.

G. Recommendations. The Traffic Impact Analysis report shall clearly state the mitigation measures recommended by the analysis and shall summarize how the recommended mitigations are roughly proportional to the identified impacts. The recommended street and highway mitigation measures shall be shown on a scaled drawing that depicts existing and recommended improvements. [ORD 4418, February 2007]

**60.55.25. Street and Bicycle and Pedestrian Connection Requirements.**  
[ORD 4302; June 2004]

1. All streets shall provide for safe and efficient circulation and access for motor vehicles, bicycles, pedestrians, and transit. Bicycle and pedestrian connections shall provide for safe and efficient circulation and access for bicycles and pedestrians.
2. The Comprehensive Plan Transportation Element Figures 6.1 through 6.23 and Tables 6.1 through 6.6 shall be used to identify ultimate right-of-way width and future potential street, bicycle, and pedestrian connections in order to provide adequate multi-modal access to land uses, improve area circulation, and reduce out-of-direction travel.
3. Where a future street or bicycle and pedestrian connection location is not identified in the Comprehensive Plan Transportation Element, where abutting properties are undeveloped or can be expected to be redeveloped in the near term, and where a street or bicycle and pedestrian connection is necessary to enable reasonably direct access between and among neighboring properties, the applicant shall submit as part of a complete application, a future connections plan showing the potential arrangement of streets and bicycle and pedestrian connections that shall provide for the continuation or appropriate projection of these connections into surrounding areas.
4. Streets and bicycle and pedestrian connections shall extend to the boundary of the parcel under development and shall be designed to connect the proposed development's streets, bicycle connections, and pedestrian connections to existing and future streets, bicycle connections, and pedestrian connections. A closed-end street, bicycle connection, or pedestrian connection may be approved with a temporary design.